

Chaplin Initiative Area Plan



Plan Adopted April 2, 2002

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Acknowledgements

This is a list of the many participants in the planning process that led to the formation of this plan. Town staff would like to extend our thanks for their participation.

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EXECUTIVE SUMMARY

Purpose

The purpose of the Chaplin Initiative Area Plan is to develop land use provisions with associated density and determine infrastructure services needed to accomplish the goals and needs for this area of Hilton Head Island. The scope of this plan is therefore focused on land use, density, and infrastructure since many of the components of development were otherwise detailed in the Ward One Master Plan adopted in 1999.

As an addendum to the Comprehensive Plan, this plan culminates the planning efforts of the Ward One Master Land Use Plan. The Ward One Plan replaced much of the Community Mixed Use (CMU) zoning district throughout the Ward One area, while setting aside the defined Chaplin Initiative Area, including its portion of the CMU zoning district, for further study.

The Chaplin Neighborhood

Generally described, the Chaplin Initiative Area is located at the center of the Island, fronting on both the Atlantic Ocean and the headwater marshes of the Broad Creek. The neighborhood of Chaplin is situated immediately to the northeast of Palmetto Dunes Resort and Hilton Head Resort (formally Four Seasons Resort). William Hilton Parkway bisects the neighborhood to the intersection of Folly Field Road and Mathews Drive. The initiative area of study then extends along the Broad Creek side of Mathews Drive and Marshland Road, terminating in the vicinity of Capitol Business Park. This project also takes into account the areas outside of the defined border of the Chaplin Initiative Area in order to coordinate land uses described by the Comprehensive Plan adopted in 1999.

Chaplin is a destination on Hilton Head Island. The most obvious and most important reasons are that it has the ocean, “the folly” tidal inlet, and frontage on the marshes of Broad Creek. This has driven change and development that otherwise would not have happened. As a consequence the development that has taken place and the pressure for continued development is more in the nature of tourism than development of a primarily residential neighborhood. This is reflected in the Plan and recommendations. The area has a high percentage of public land that can be used as an amenity to compliment private land. In addition to public land there are four different zoning classifications.

Recommendations & Implementation Strategies

1. Land Use Regulations

The Chaplin Initiative Area Plan proposes to shape land uses towards a mix of more efficient residential, commercial and tourism development patterns. Proposed land uses take advantage of the strategic location of Chaplin; served by major and minor arterial roads with land fronting along the Atlantic Ocean and uninterrupted views of the marshes

of Broad Creek. The net effect is that this Plan confirms some existing development, such as public lands, while creating new zoning districts to allow ocean and marsh oriented development at an appropriate scale for Hilton Head Island.

2. Infrastructure Improvements

The Ward One Master Land Use Plan specifically designated the Chaplin Initiative Area for study of its infrastructure needs. Investments by the Town and private developers will be targeted for revitalization to enhance economic development opportunities while improving community character and facilitating safe, efficient traffic circulation. The Chaplin Initiative Area Plan takes into account the current public and private facilities and proposes to enhance current areas of concern to an acceptable level for Hilton Head Island. The plan calls for new circulation roads; roadway improvements; pathways; intersection improvements; pedestrian facilities at road crossings; and installation of sewer service. In addition, passive and active recreational parks are proposed on public lands.

3. The Chaplin/ Marshland/ Gardner Property Owners Association

Throughout the development of the Chaplin Initiative Area Plan, the Chaplin/ Marshland/ Gardner Property Owners Association (POA) provided a forum for the exchange of ideas and opened lines of communication between the Town staff and the Chaplin Initiative Area property owners and stakeholders. This effort by the POA proved vital to the development of this Plan. The Plan views the continued relationship between the POA and the Town of Hilton Head Island as an excellent vehicle to strengthen the ability of the neighborhood to communicate its needs for future public project improvements. Also, this Plan proposes to develop the POA/ Town relationship to mutually support the goals this Plan describes.

Chapter 1. INTRODUCTION

Purpose of Study

The purpose of this Plan is to determine the appropriate land use, density and infrastructure improvements for the Chaplin Initiative Area. This Plan outlines and provides general guidance in keeping with the Town of Hilton Head Island's Comprehensive Plan and the implementation of any governmental provisions, such as ordinances, while including guidance on issues facing the entire Island community.

The Chaplin Initiative Area Plan proposes to shape land uses towards a mix of more efficient residential, commercial and tourism development patterns while enhancing the character and natural amenities of the Chaplin Neighborhood. Proposed land uses take advantage of the strategic location of Chaplin; served by major and minor arterial roads with land fronting the Atlantic Ocean and provides uninterrupted views of the marshes of Broad Creek. The net effect is that this Plan affirms some existing development, such as public lands and platted subdivisions, while creating new zoning districts to encourage ocean and marsh oriented development at an appropriate scale for the Island community.

Study Area Location

The Chaplin Initiative Area is generally located in the middle of Hilton Head Island (See Figure #1, *Chaplin Initiative Area Location Map*). The study area is best described as being north and east of the Palmetto Dunes Resort to William Hilton Parkway's intersection with Mathews Drive and Folly Field Road. Then the study area continues along the Broad Creek marshside of Mathews Drive and Marshland Road, terminating in the vicinity of Capitol Business Park. The Chaplin Initiative Area therefore fronts the headwaters of the Broad Creek. Additionally, a significant 4,100 linear feet of the study area fronts the Atlantic Ocean, which includes a large tidal inlet, locally referred to as "*The Folly*." As a defined area of Hilton Head Island, there is no other neighborhood that contains these characteristics.



Chaplin Initiative Area

Location Map

- Main Roads
- Parcel Lines
- Area Identity**
- Open Water
- Salt Marsh
- Upland



800 0 800 1600 Feet

April 2, 2002

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History Leading to the Chaplin Initiative Area Plan

This Chaplin Initiative Area Plan is the culmination of almost two decades of general planning processes dating back to the incorporation of the Town of Hilton Head Island in 1983. Prior to the existence of the Town of Hilton Head Island, the Chaplin Area was a community node for a mix of residential and neighborhood commercial activity. Development occurred on an opportunity basis with development regulations administered by Beaufort County. Upon incorporation as a town, the municipal government of Hilton Head Island embarked on comprehensive planning efforts and the implementation of land use regulations that were more stringent than the development regulations of Beaufort County.

In 1985, and later updated in 1991, the Town adopted a Comprehensive Plan that generally described the northern Island areas outside of the Planned Unit Developments (PUD's) as in transition and in need of further study. The Town of Hilton Head Island's Land Management Ordinance (LMO), originally adopted in 1987, prescribed design and performance standards that rendered some developments nonconforming, making redevelopment difficult. In 1993, the Town embarked on a planning effort designated the 'Ward One Master Land Use Plan' to address those transitional areas of the Island.

The Ward One Master Land Use Plan was adopted, almost simultaneously with the updated Comprehensive Plan, in 1999. The Ward One Plan went farther than any previous plan to direct the development of that area of the Island located outside of the PUD's. That plan also identified two "Initiative Areas" where additional planning and negotiation were needed prior to any recommendations of future land use, density and infrastructure could be made. The "Chaplin Initiative Area" is one of these two neighborhoods identified by the Ward One Master Land Use Plan. The second neighborhood is located at the gateway to Hilton Head Island and referred to as the Stoney Community, which is the subject of a similar planning effort.

Since its designation, the Chaplin Initiative Area has proceeded with a planning process to gather information and input from many sources, conducted a neighborhood charette to solidify consensus and then created this Plan and prepared it for adoption as an addendum to the 1999 Comprehensive Plan. A culminating point in this planning effort was the Town-sanctioned Chaplin Initiative Area Neighborhood Charette, June 7-10, 2000. This charette was open to the public at large, however, only property owners and members of the Chaplin/ Gardner/ Marshland Property Owners Association were specifically sent mailed invitations. The South Carolina Downtown Development Association (SCDDA), a non-profit firm located in Columbia, South Carolina, provided facilitation services and expertise in gathering input from the charette attendees. SCDDA further provided a report of the charette findings that serves as a resource of public input in this community effort to master plan the Chaplin Area.

To summarize, this Plan is an addendum to the current Comprehensive Plan adopted in 1999, and also fills the role as a completion of the Ward One Master Land Use Plan, likewise adopted in 1999. The planning process invoked for both the Comprehensive Plan and Ward One therefore led to the creation of this Chaplin Initiative Area Plan.

Chaplin Initiative Area Vision

The ultimate goal of this Plan is to provide for a high quality of life by planning for population growth and private redevelopment. This must occur through the proper distribution, location and intensity of land uses with adequate levels of services while maintaining and protecting the natural resources, the residential neighborhood and the overall character of the Island.

Inherent in this land use vision for the Chaplin Initiative Area is the concept of sustainability. Sustainable development is a strategy by which communities identify planning development approaches that also protect and promote natural environment and quality of life. The goals of the land use vision are intended to support the following concepts:

1. Sustainable balance of land uses
2. Sustainable balance between human activity and natural resources
3. Sustainable balance between land use and public infrastructure and services

Chapter 2. The Chaplin Initiative Area Neighborhood

Existing Conditions and Constraints to Development

Land Use

The existing mix of residential and commercial uses in the study area began prior to the incorporation of the Town of Hilton Head Island. The current zoning districts of the neighborhood are a mix that essentially confirms the existing development pattern. An inventory of the existing development in the Chaplin Initiative Area details this mix of uses. (See Figure #2, *Existing Land Use*).

Table #1 – Residential Development

Residential--87 Acres of the 372 acres

Single Family Units	Multi-Family Units	Manufactured Home Units
87	0	23

Source: Town of Hilton Head Island Planning Department, April 2000 Data

Residential uses are found throughout the area. Single Family Dwelling Units exist on formally platted subdivisions along Bradley Beach Road, Burkes Beach Road and Singleton Beach Road. Pepered along the marshes of Broad Creek and somewhat between the platted subdivisions, family developments exist in unplanned subdivisions. In many cases, single family houses and family developments access directly to the main roads and create a multitude of curb cuts along William Hilton Parkway and Marshland Road.

Table #2 – Non-Residential Development

Commercial – 69.5 Acres of the 372 acres

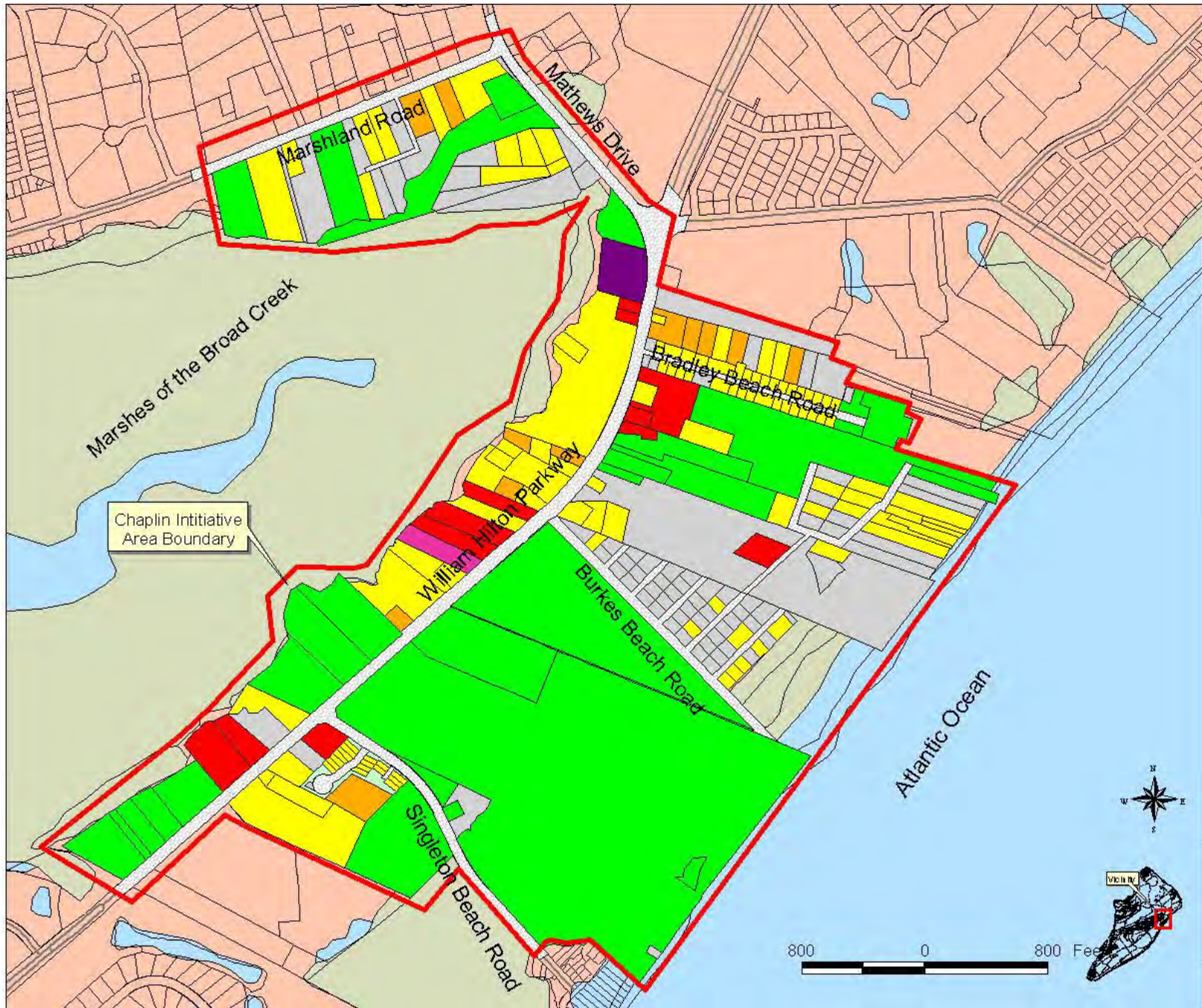
Retail Sq. Ft.	Office/ Bank Sq. Ft.	Other Comm. Sq. Ft.
21,900	13,000	23,200
Other Non-Residential Uses – 163.5 Acres of the 372 acres		
Parks/ Publicly Owned Acres		Cemetery Acres
161		2.5

Source: Town of Hilton Head Island Planning Department, April 2000 Data

Commercial development exists almost exclusively along William Hilton Parkway. These businesses include neighborhood scale restaurants, a seafood vendor, a part-time produce stand, an antique furniture store, a liquor sales store and three convenience stores with gas sales. Also included in the commercial mix, there exists a used car sales operation, a wholesale/ retail paint store and a construction site clearing company. While

all of these non-residential uses may have changed ownership over the years, they all are part of the longstanding commercial services offered at these locations.

Worth mentioning, there are approximately 52 privately owned vacant acres (large parcels with a few homes clustered in one area) interspersed throughout the study area. However, there is a concentration of vacant acreage oriented towards the Atlantic Ocean and along Marshland Road. (See Figure #2, *Existing Land Use*).



Chaplin Initiative Area

Existing Land Use

- Landuse Descriptions**
- Single Family
 - Multi-Family
 - Retail/Commercial
 - Office/Commercial
 - Light Industrial
 - Manufactured Home
 - Open Space/Public
 - Open Space/Private
 - Institutional
 - Vacant
 - Right-of-Way
 - Main Roads
 - Parcel Lines
- Area Identity**
- Open Water
 - Salt Marsh
 - Upland

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Zoning

Six zoning districts exist in the Chaplin Initiative Area and reflect the current land use of the neighborhood (see Figure #3, *Existing Zoning*). They are identified as the RM-8, *Residential Moderate Density District*; the CMU, *Community Mixed Use District*; the RD, *Resort Development District*; the CL, *Light Commercial District*; the PR, *Parks and Recreation District*; and the CON, *Conservation District*. Following is the 'Use of Character and Purpose Statements' for each zoning district with a summary of the types of uses allowed.

RM-8 -- *Residential Moderate Density District*

It is the intent of this district to allow the development of residential uses up to eight dwelling units per net acre. This district is used to encourage a moderate density neighborhood providing a variety of residential opportunities for residents of the Town.

Uses allowed in RM-8 include residential (single and multi-family); and limited public and civic such as parks and religious institutions (other institutions, cemetery, and minor utility by special exception). Commercial, Industrial, Agricultural and Water-oriented uses are not allowed.

RD -- *Resort Development District*

- A. It is the intent of this Resort Development District to provide for tourist resort development in the form of multifamily, timeshare or interval occupancy units intended for use as resort transient lodging, and, under controlled circumstances, the development of motels and resort hotels.
- B. It is also the intent of this district to provide for commercial development aimed at serving the transient Island visitor. The commercial development is meant to service primarily the market created by the needs and desires of the transient population staying in the residential resort area.

Uses allowed in the RD include residential (except manufactured housing park); public and civic such as government facilities, parks, minor utilities and telecommunications facilities (major utilities, community parks and other institutions by special exception); many commercial uses, primarily those that serve the tourism industry (some intense uses such as hotels, interval occupancy, gas sales and convenience stores are by special exception). Industrial, Agricultural and Water-oriented uses are not allowed.

CL -- *Light Commercial District*

The Light Commercial District is intended to allow development of light intensity, low turnover, commercial uses where environmental, aesthetic or traffic concerns are significant.

Uses allowed in the CL district include limited residential for single-family (multi-family and second floor residential by special exception); limited public and civic for government facilities, religious institutions, neighborhood parks, minor utilities and telecommunications facilities (major utilities and community parks by special exception); limited commercial for uses such as offices, banks, convenience stores, gas sales and retail sales and service that is low turnover; and agricultural uses. Industrial uses are not allowed.

CMU -- Community Mixed Use District

- A. It is the intent of the Community Mixed Use District to encourage development in a flexible manner in areas where transition is imminent but the direction has not yet been manifest, or in areas of sparse residential settlement, but which area would reasonably be expected to begin to mature in the near future.
- B. It is intended that this district be utilized over large areas of land in order that the economic market place be allowed to assert itself in a positive manner.
- C. With the exception of the Stoney Community, which has developed as a mix of commercial, light industrial and residential uses, this district is primarily residential in nature and is not intended to allow heavy commercial or manufacturing uses.
- D. The Stoney Community shall be defined as that area fronting on both sides of U.S. Hwy. 278 bounded on the east and south by the marshes of Jarvis Creek, on the west by the marshes that separate Hilton Head Island from Jenkins Island and on the north by the marshes of Skull Creek or a distance of 1500 feet north of the centerline of U.S. Hwy. 278 which ever is closer to U.S. Hwy. 278. In all cases the marsh boundary line is co-terminus with the boundary line of the CON Conservation/Parks/ Open Space District.
- E. This district (with the exception of the Stoney Community) is intended to provide self-contained areas that are separated from the main elements of the Island transportation network such that their growth under this district will remain internal, and therefore contribute to the minimization of vehicular trips on major thoroughfares. Such minimization will be encouraged by the provision of goods and services to the residents of the area from within the district.
- F. It is contemplated that land in this district will be monitored through time and that, if land use conflicts begin to occur, further planning and rezoning to specific districts will take place. The Town will work with neighborhood residents and owners in order to adopt a neighborhood plan as a basis for more permanent land use districts than the CMU District.

The uses allowed in the CMU district include residential (single and multi-family); public and civic (such as day care, schools, government facilities, churches, parks, and utilities); some commercial (such as restaurants, offices, bed & breakfasts, banks, and some stores); some industrial (mostly by special exception); agriculture and marina (by special exception).

PR -- Parks and Recreation District

- A. The intent of this Parks and Recreation District is to manage the types of land uses permitted on publicly held land by permitting the establishment of areas within the Town for active or passive recreation, or providing for the preservation of land in its natural character for public enjoyment.
- B. Development within this district shall be designed to minimize, as much as possible, the impact on both the environment and community.

Uses allowed in the PR district are primarily parks and cemeteries. Minor utilities and some water-oriented uses are also allowed. Major utilities and telecommunications facilities are subject to special exception. No other uses are allowed in the PR district.

CON -- Conservation District

The purpose in establishing this conservation district is to regulate very low intensity development in environmentally sensitive tidal wetland areas and the beach. Only development which will minimally disrupt natural features or systems, whether temporarily or permanently, will be allowed.

Uses allowed in the CON district are parks, utilities and other water-oriented uses.



Chaplin Initiative Area

Existing Zoning

Zoning Districts

CC	Commercial Center
CL	Parkway Light
CMU	Community Mixed Use
CON	Conservation
IL	Light Industrial
OL	Office/ Institutional Lt. Intensity
PD-1	Planned Development
PR	Parks, Rec., & Public Facilities
RD	Resort Development
RM-4	Low Density
RM-8	Moderate Density
RS-2	Detached Single Family
RS-3	Detached Single Family
RS-4	Detached Single Family

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Density

The following table depicts the density provisions by each zoning district that effect the Chaplin Initiative Area.

Table #3: Permitted Density

Permitted Density			
District	Maximum Residential Density By Right(per net acre)	Maximum Hotel/ Motel/ Inn Density By Right (per net acre)**	Maximum Nonresidential Density By Right (per net acre)
RM-8	8 DU	--	--
RD	8 DU	20 Rooms	8,000 SF
CL	2 DU	--	7,000 SF
CMU	4DU*	--	7,000 SF
PR	--	--	6,000 SF

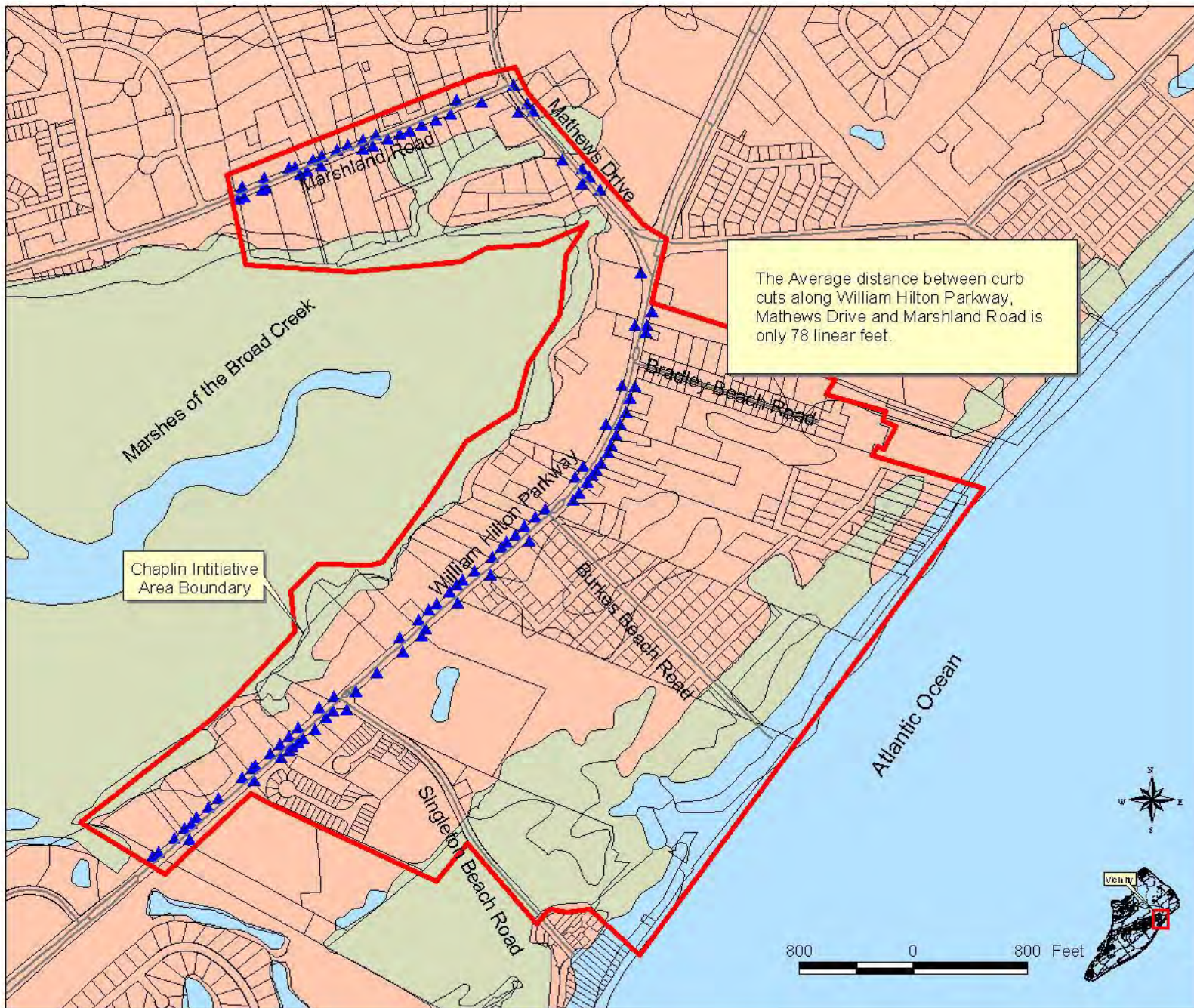
Source: Town of Hilton Head Island Land Management Ordinance Sec. 16-4-801. Density Standards Table

* **NOTE:** Maximum residential density in the CMU District within 500 feet of the OCRM Critical Line shall be 8 dwelling units per net acre, except within 500 feet of the Broad Creek OCRM Critical Line where the maximum shall be 6 dwelling units per acre.

** **NOTE:** Hotel/Motel/Inn rooms utilized as interval occupancy units are considered as Residential Density.

Traffic Conditions

There are a number of general conditions in the study area that cause problems for the safe and efficient flow of traffic. The area has several large capacity roads and virtually no sub-collector or access roads. By its very nature, the mix of uses encourages traffic throughout the day, not to mention the area's location facilitates through traffic. Of particular note, there are many curb cuts onto the arterial roads that do not meet current the standards of the LMO. (See Figure #4, *Curb Cut Locations*).



Chaplin Initiative Area

Curb Cut Locations

The Average distance between curb cuts along William Hilton Parkway, Mathews Drive and Marshland Road is only 78 linear feet.

- Main Roads
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As described earlier, three arterial roads, William Hilton Parkway, Mathews Drive and Marshland Road directly serve the Chaplin Initiative Area. William Hilton Parkway effectively bisects development along the marshes of Broad Creek from land uses on the Atlantic Ocean side of that major arterial. This section of William Hilton Parkway is a four-lane highway with a paved median. Prior to the construction of the Cross Island Parkway, all vehicular traffic from the southern end of the Island channeled through the Chaplin Area on its way to the mainland. This section of William Hilton Parkway saw a traffic count rate between 44,000 and 50,700 Average Daily Trips (ADT) during June of 1997, prior to the construction of the Cross Island Parkway. As of June 2000, this section of William Hilton Parkway experiences between 35,800 and 43,000 ADT. Despite the 7,700 drop in ADT from 1997 to 2000, this is still a significant portion of the traffic on the Island.

Mathews Drive is considered a minor arterial road. The section of Mathews Drive located within the study area is a four lane road without a median. One family residential development exists off of Mathews Drive, consisting of approximately six residences with a single curb cut. This section of Mathews Drive is a conduit for vehicular traffic from many mid-Island locations to William Hilton Parkway.

The portion of Marshland Road within the Chaplin Initiative Area not only delineates the study area boundary but also defines the separation of major land uses. So, like William Hilton Parkway and Mathews Drive, there is a problem of constant flow of traffic throughout the day. In the study area and fronting along the marshes of Broad Creek, single-family structures and undeveloped lots exist. Surrounding this section of the initiative area are light industrial uses and wholesale businesses. Like Mathews Drive, Marshland Road is a minor arterial road; it serves vehicular traffic from development along the Broad Creek and to a lesser degree acts as an alternate route for traffic heading north-south on the Island.

Vehicular access to the arterial roads from developments in the Chaplin Initiative Area is primarily via individual curb cuts, as opposed to a grid system of collectors or sub-collectors. Over time, the individual curb cuts occurred prior to the incorporation of the Town and the 'Access to Streets' provisions currently employed by the LMO. For instance, curb cuts along William Hilton Parkway are intended to be 500 linear feet between centerlines of access points, 200 linear feet for Mathews Drive and Marshland Road. Figure # 4, *Curb Cut Locations*, demonstrates that the majority of curb cuts onto these arterial roads are not in compliance with today's LMO. The average distance between curb cuts along the arterial roads is only 78 linear feet. While there are existing approximately 18 curb cuts that no longer serve any development and need to be removed, their removal alone does not solve the number of curb cut problems or existing developments with multiple curb cuts. This problem exacerbates the safe vehicular traffic flow in the study area.

The speed of traffic was listed as a concern during the creation of the Ward One Master Land Use Plan. However, during the Chaplin Initiative Area Charette the residents did not single out the issue of the speed of the vehicular traffic as a particular problem as

much as it was identified as a contributor to unsatisfactory safety and efficiency conditions. Through traffic has the right-of-way and typically this type of traffic is easily accommodated. The problems arise when vehicles are entering the arterial roads from the many curb cuts and have to quickly get up to speed to join traffic. Whether executing a right or left turn motion, either situation is less than ideal. Inefficient and unsafe conditions arise when vehicles try to exit the arterial roads onto the small or inadequately sized curb cuts. There also exist situations where curb cuts have inadequate line of sight, called site triangles, to the traffic on the arterial roads, such as the intersection of Candy Doll Bluff and William Hilton Parkway.

Contributing to the many individual curb cut problems, vehicle access between developments is virtually non-existent. This situation causes vehicles to enter an arterial road in order to drive to the development or residence that is adjacent. A great example would be a scenario where a vehicle leaves the 'General Store' en route to visit 'Barnacle Bill's Seafood' next door. The two developments are adjacent to one another and front on William Hilton Parkway; further, their curb cuts are only 50 feet apart. A conclusion of this study is that access between developments, the reduction of curb cuts on arterial roads, and the lack of aligned access points need to be addressed.

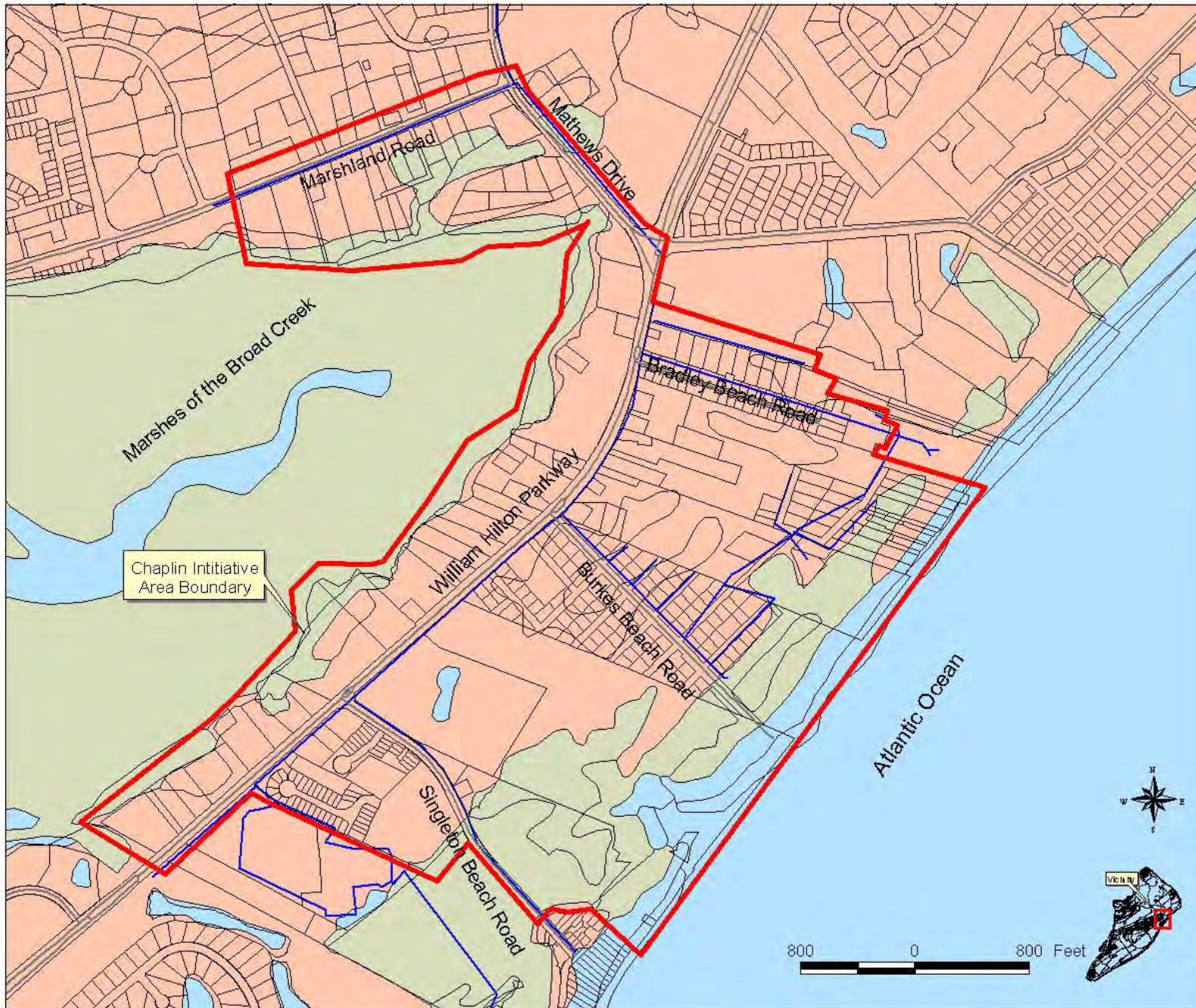
Other Infrastructure

Water Service

Water service is located throughout the study area. There is adequate service to meet the current need and water supply is available for the future needs described in this Plan. The issue of water service is not considered a pressing need or a constraint to development. (See Figure #5, *Existing Water Lines*).

Fire Protection

The fire protection provided by fire hydrants has recently become a constraint for public safety and redevelopment. The State of South Carolina has mandated that municipalities adopt the 2000 International Fire Code to regulate fire protection in their jurisdictions. Figure #6, *Existing Fire Hydrant Locations*, depicts the current location of fire hydrants in the study area. While these provide a measure of fire safety and for the most part meet current fire code provisions, they are not spaced to provide the level of service required by the 2000 International Fire Code, which is scheduled to go into effect in the Town of Hilton Head Island in July 2001. This new code requires a maximum distance of 400 feet from a hydrant to any new structure, or 600 feet for single family residences (including manufactured and stick built). The method of measurement difference from the current code compared to the 2000 International Fire Code also plays a significant role in the impact of the new code. The current Fire Code uses a 500 linear feet radius from a fire hydrant to the property line (or as the crow flies) to determine if a property is in compliance. The 2000 International Fire Code measures the distance from a fire hydrant that the route of the hose has to take. Meaning some situations likely cause the hose to be routed around existing structures and fences. Therefore, it is anticipated that this new fire protection standard will require the installation of additional fire hydrants.



Chaplin Initiative Area

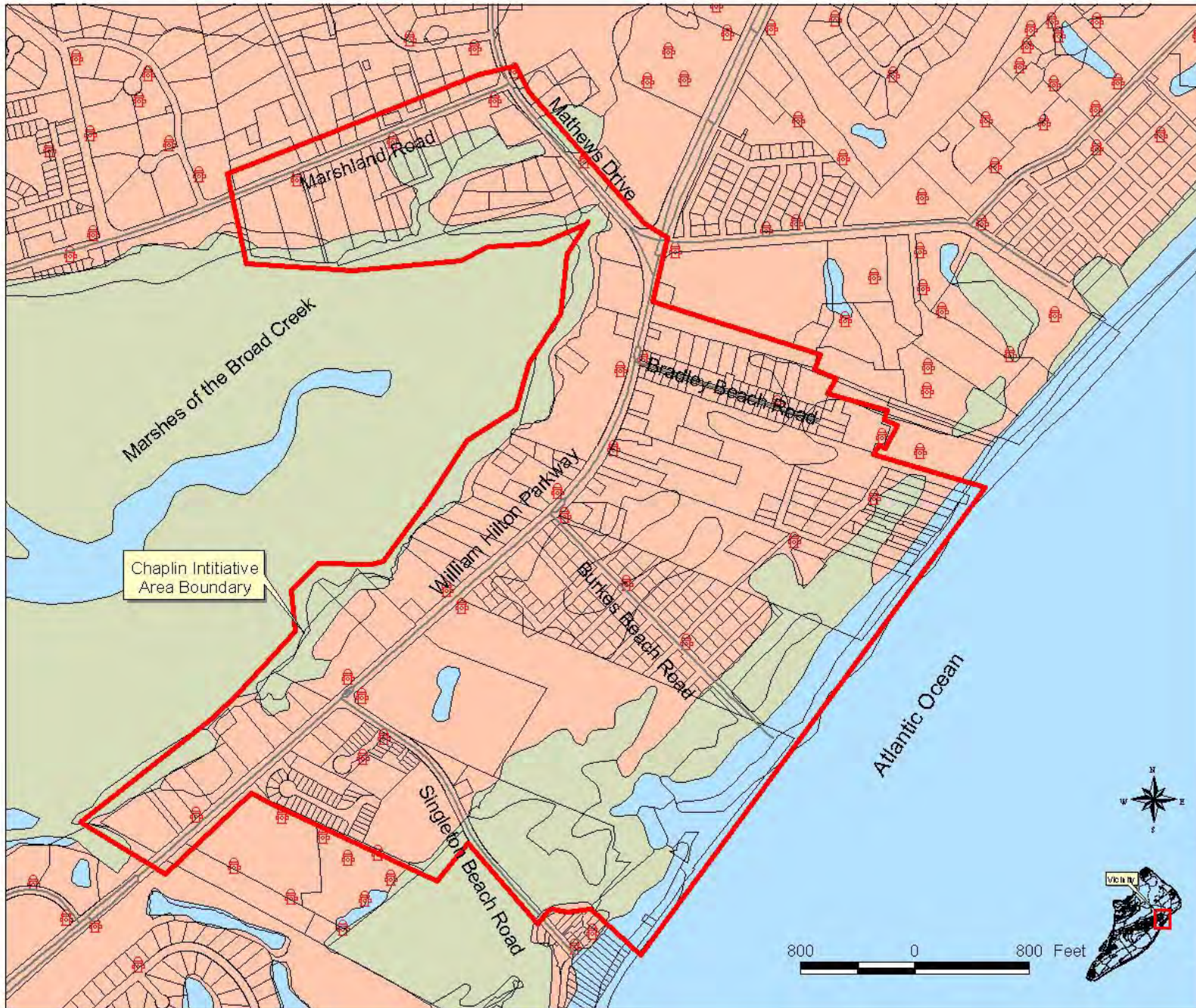
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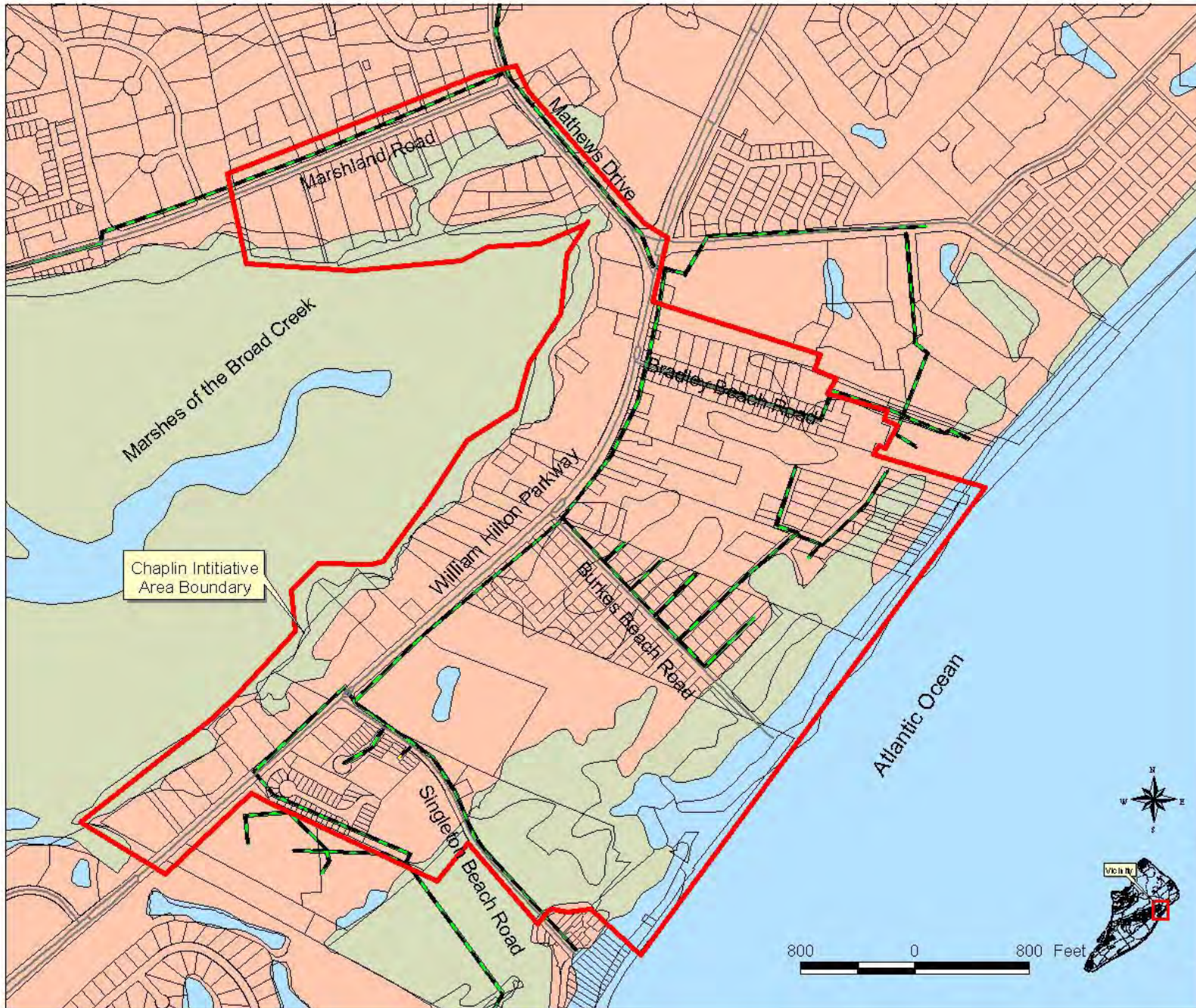
Figure # 6

Sewer Service

Sewer service is more sporadic than water service. As planned subdivisions were created, the sewer service was installed at the owner or developer's expense. (See Figure #7, *Existing Sewer Lines*). Hence, sewer service is provided to the planned subdivisions and a few of the businesses; otherwise septic tanks are widely employed as the means of waste water treatment. The Ward One Master Land Use Plan describes the use of septic tanks in the study area as a health and safety concern based on soil conditions and high water tables.

Power and Cable Service

Electric power is available throughout the study area, and an assumption is made that Cable Television is also available throughout the area. Neither of these services is considered to be a pressing need nor problem for future development. However, overhead lines are unsightly and might be a problem after a disaster such as a hurricane. Burying the lines could provide for a speedier recovery and improve the appearance of the neighborhood.



Chaplin Initiative Area

Existing Sewer Lines

- Main Roads
- Parcel Lines
- Sewer Lines
- Area Identity**
- Open Water
- Salt Marsh
- Upland

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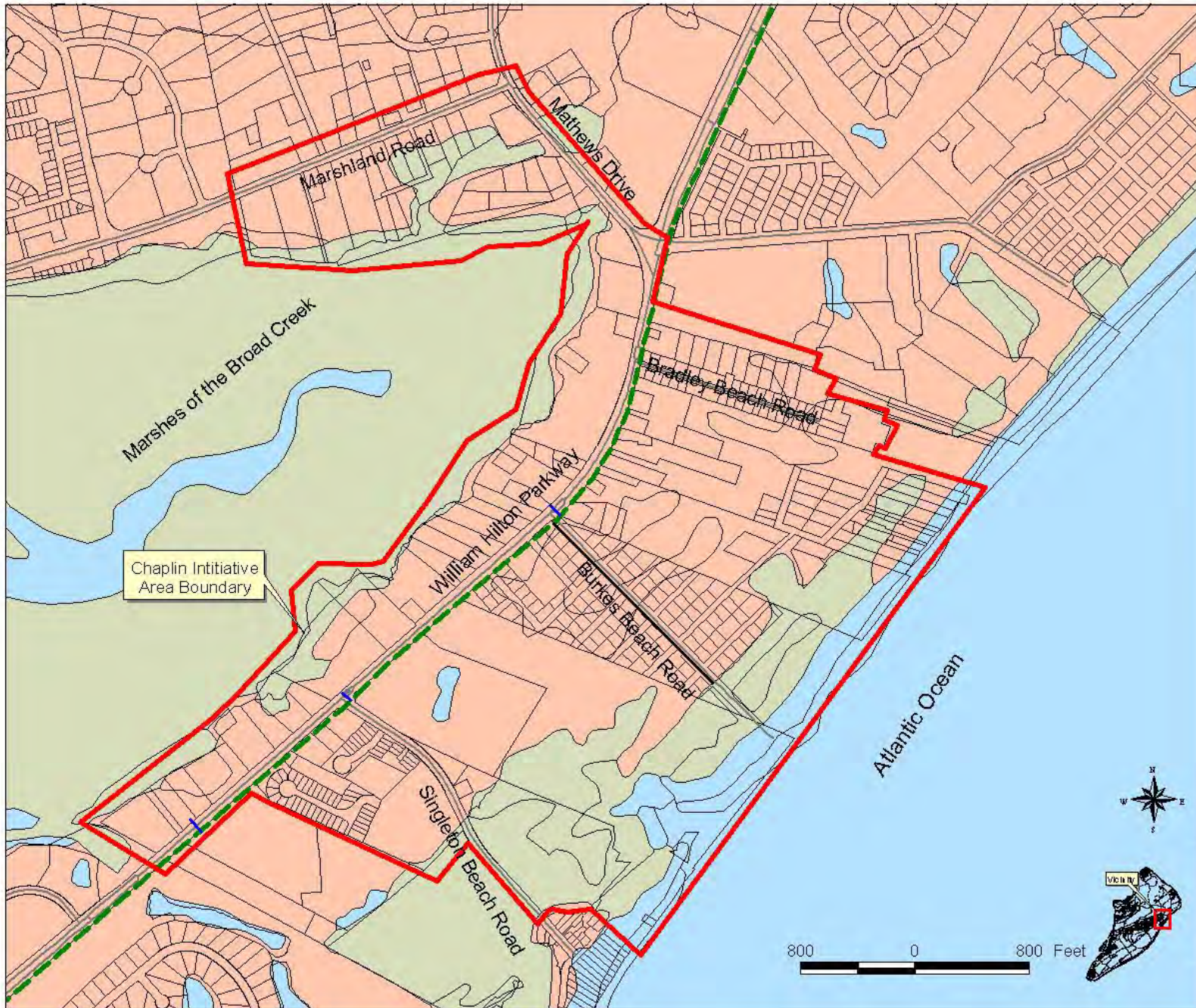
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Pedestrian Facilities

In general, the pedestrian facilities throughout the study area are inadequate to meet the needs of pedestrian transportation. Pedestrian facilities primarily take the form of a six-foot wide, concrete sidewalk abutting the travel lanes of William Hilton Parkway, causing an uncomfortable situation for pedestrians being close to traveling vehicles.

There are three painted, pedestrian crosswalks in the study area (one each at the William Hilton Parkway intersections with Burkes Beach Road; Singleton Beach Road; and the entrance of Hilton Head Resort). (See Figure #8, *Existing Pedestrian Facilities*). An eight-foot wide, multi-purpose trail parallels the east side William Hilton Parkway through the neighborhood, this trail is usually separated by at least six feet from the concrete sidewalk, but sometimes abuts or overlays the sidewalk. Also, there is a four-foot wide bike lane adjacent to the southern side of Burkes Beach Road for its entire length, from William Hilton Parkway to the beach. There are no pedestrian friendly traffic signals or intersections.

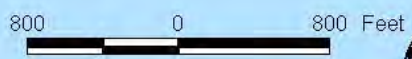


Chaplin Initiative Area

Existing Pedestrian Facilities

- Pedestrian Facilities**
- 8-foot wide Pathway
 - Painted Crosswalk
 - 4-foot wide Bike Lane
 - Main Roads
 - Parcel Lines
- Area Identity**
- Open Water
 - Salt Marsh
 - Upland

April 2, 2002



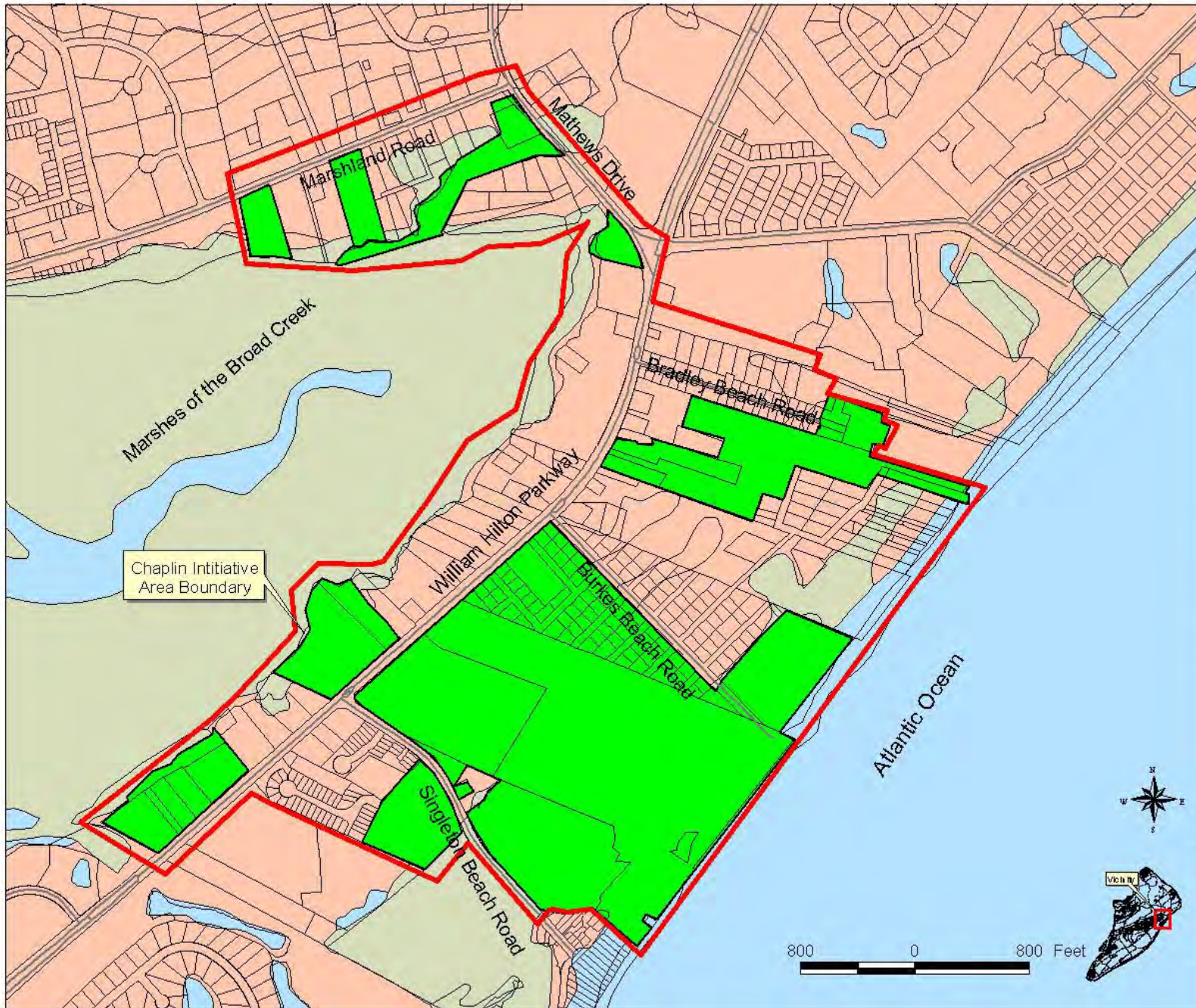
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Parcel considerations

There are several planned residential subdivisions in the Chaplin Initiative Area; they are located along Bradley Beach Road, Burkes Beach Road and Singleton Beach Road. Otherwise, the majority of properties are remnants of family plots subdivided randomly over time. These remnant plots typically take a linear form with a short frontage along the arterial roads, apparently in an effort to allow frontage along the roads while extending the properties in equal shares to other amenities, such as a marsh or ocean frontage. The net effect is that many underdeveloped properties exist along arterial frontage that would have difficulty redeveloping currently based on LMO design and performance standards, such as setbacks and buffers or access to roads. Figure #9, *Parcel Considerations* depicts the current alignment of parcels and Town-owned property.

Town-owned property is extensive in the Chaplin Area. At this time, most of these tracts are undeveloped (148 acres out of 161 acres). Plans for a recreation complex on the Ferguson/ Singleton Tract are in a final stage, as are plans for Beach Access parks on the Malphrus Tract and Collier Beach. There is the Driessen Beach Park at the end of Bradley Beach Road and an informal beach access point at the end of Burkes Beach Road. Future uses of the Town-owned properties along the Broad Creek frontage are described in Chapter 5, Implementation Strategies.

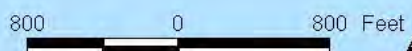


Chaplin Initiative Area

Parcel Considerations
Town-owned Parcels

-  Main Roads
-  Parcel Lines
-  Town-owned Land
-  Open Water
-  Salt Marsh
-  Upland

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Land Development Regulations

Current land use regulations stipulated in the LMO are a constraint to redevelopment in the Chaplin Initiative Area, primarily due to the fact that much of the area developed on an opportunity basis under less stringent guidelines. Said simply, the rules have changed leaving many of the existing developments in a nonconforming status. As it was touched upon above in ‘Parcel Considerations,’ simple design and performance standards such as setbacks, buffers and even access to roads may make it difficult for some properties to redevelop.

Natural Features

The Chaplin Initiative Area has natural features that make the area stand out as unique on Hilton Head Island. Land fronting along the beaches of the Atlantic Ocean, the large natural inlet and marshes of ‘The Folly’ and the marshes of the Broad Creek are nice amenities. Yet there are some natural features in the study area that have a considerable effect on the redevelopment that can occur. Land development regulations cited earlier also apply to natural resources, such as buffers from saltwater marshes or setbacks from the Office of Coastal Resource Management dune baseline.

The topography of the study area also plays a role in land usage. Regarding elevation, the Chaplin Initiative Area ranges from around 4 feet above sea level (ASL) to 12 feet ASL. According to Federal Emergency Management Agency (FEMA) guidelines, the entire study area is subject to flood zone regulations that require structures to meet minimum floor elevations between 14 to 18 feet ASL. In addition, certain building types and locations may require the filling of sites to meet FEMA regulations, where other buildings and locations can be built on piers. Meeting the minimum FEMA floor elevation requirements alone can have a profound effect on the cost, location, building type and certainly the aesthetics of a development. Another challenge of the area’s topography is the relationship between the marshes, the Folly, and the ocean and the role that they can play during a catastrophic flood situation, such as a hurricane. So, the potential susceptibility to disaster has an effect on the way that development can occur in the study area.

Chapter 3. Opportunities for Redevelopment

To date, the Chaplin Initiative Area development pattern has not fully taken advantage of the potential for development provided by even its current zoning provisions, because several of the existing conditions mentioned above are directly tied to the development that has taken place. There are soil conditions and lack of sewer service in the entire area that lead to the relatively low density. Traffic and access to arterial roads effected the current development pattern. The area is not considered pedestrian friendly either. And finally, as one of the original communities on the island, there are aesthetic considerations of some of the older structures that could bring the area more up to date with the rest of the Island community.

It is recognized that the existing redevelopment conditions in the Chaplin Initiative Area are in need of ways to address its health and safety issues as well as improve aesthetic conditions from its roadways, marsh fronts and oceanfront. The best way to deal with its issues is to further recognize that the Chaplin Initiative Area enjoys a combination of natural amenities and potential as an Island amenity open to the public at large.

The Chaplin Initiative Area fronts along the headwaters of the Broad Creek with unspoiled view sheds of the entire length of the marshes south to the Fraser Bridge. Currently, this area is zoned CL - *Commercial Light* and CMU - *Community Mixed Use*. It developed as a mix of family residential developments with pockets of commercial uses; the Town also owns several parcels of underdeveloped contiguous properties along this marsh frontage.

The area's greatest asset for potential as an Island amenity is its $\frac{3}{4}$ mile Atlantic Ocean frontage, including The Folly. Zoning districts in this part of the study area are RM-8, *Residential Moderate Density*; RD – *Resort Development*; CON – *Conservation District*, and PR – *Parks and Recreation*. While several residential subdivisions are already platted and developed, there still exist large undeveloped tracts with ocean frontage, almost all of which are owned by the Town and slated for a mix of active or passive parks. There remains one large privately owned tract (Burkes Family Property) that is considering a mix of timeshare and commercial uses.

With these assets in mind, and the need to improve the health, safety and welfare of the area, this plan proposes several changes. Restaurants and shops with residential uses above could orient towards the marsh views while resort accommodations and single family homes enjoy their oceanfront orientation. Interspersed in this mix of future uses, this plan further proposes an interconnected system of passive and active parks that serve not only neighborhood scale needs but also Island-wide recreational needs as well. Further, this plan proposes a host of pedestrian friendly facilities, such as boardwalks, pathways, crosswalks throughout the area, and a pedestrian overpass across William Hilton Parkway. The introduction of new interior access roads and the elimination of most curb cuts onto the arterial roads would benefit traffic. Safe and efficient transportation can be established for both vehicles and pedestrians.

Chapter 4. NEEDS AND GOALS

This chapter details specific goals and needs to reach the future vision described on page 4, *The Chaplin Initiative Area Vision*. With the understanding of the existing conditions and constraints to health, safety, and welfare, and realizing the opportunities to correct these deficiencies, the following needs identify the problems and the goals serve as solutions for those problems.

Land Use and Density

Need 1: Some of the current zoning districts no longer meet the needs or desires of the Chaplin Initiative Area residents or the Island community. The Chaplin Initiative Area could be a destination on Hilton Head Island that serves both visitors and residents.

Goal 1a: **Provide for compatible development in the Chaplin Initiative Area with an appropriate balance of land uses that do not adversely impact the natural resources of the entire Island community.**

Goal 1b: **Direct redevelopment in areas of the Chaplin Initiative Area which provide appropriate infrastructure capacity to accommodate growth in an environmentally acceptable manner based on the Comprehensive Plan and this Chaplin Initiative Area Plan.**

Need 2: Current land use regulations make redevelopment of the Chaplin Initiative Area difficult since the existing land use and property configurations developed prior to current standards.

Goal 2a: **Address and promote redevelopment and infill in the Chaplin Initiative Area for a more efficient use of land.**

Goal 2b: **Town ordinances should establish flexible redevelopment provisions in the LMO to ensure that redevelopment is not discouraged in the Chaplin Initiative Area.**

Goal 2c: **The Town should establish LMO provisions for the recombination of parcels consistent with this Plan to allow for appropriate access and developable area.**

Goal 2d: **The town should research the ability to move density rights from within the Chaplin Initiative Area boundary to the RD zoned properties also within the Chaplin boundary.**

Need 3: The Chaplin Initiative Area existing land use pattern discourages a mix of uses suitable for this part of the Island community.

Goal 3a: Allow an Island pedestrian amenity that allows the enjoyment of a mix of uses for residents and guests to the Chaplin Initiative Area.

Goal 3b: The Town should encourage the creation of a mix of uses for the public enjoyment of parks, residential and commercial activity that is at an appropriate scale for the Island community.

Infrastructure

Need 4: The area needs alternate routes to provide better local access to and between developments that is safe and minimizes the number of curb cuts along William Hilton Parkway, Mathews Drive and Marshland Road.

Goal 4a: The Town should investigate and create alternate routes that parallel William Hilton Parkway and provide local access with a minimal number of curb cuts.

Goal 4b: The Town should encourage redevelopment to provide vehicular access between developments.

Need 5: Roadway aesthetics along William Hilton Parkway and Marshland Road need to be improved. It is recognized that the combination of traffic volume, paved medians, high number of curb cuts and lack of landscaping cause not only a dangerous situation but also detracts from the potential beauty of the Chaplin Neighborhood.

Goal 5: The Town should improve aesthetic quality of William Hilton Parkway and Marshland Road with a series of roadway improvements such as the burying of power lines; closing curb cuts; providing pedestrian lighting; and landscaping medians.

Need 6: The William Hilton Parkway intersections with Burkes Beach and Singleton Beach Roads, and the intersection of Mathews Drive/ Marshland Road may require traffic signals. Further, any new roads that are created should line up with the existing intersections, causing even more need to study the signaling of major intersections and encourage development patterns that agree with this Plan.

Goal 6: The Town should work closely with the South Carolina Department of Transportation to conduct Signal Warrant Studies proactively and determine the best location of signalized intersections ahead of future redevelopment.

Need 7: Existing pedestrian facilities are inadequate to encourage people to get out of their vehicles and move around the Chaplin Neighborhood. The neighborhood should be a part of the Island community that is safe for pedestrians. This will lead to better use of the area as an Island amenity

and provide safer ways to allow for the mobility of people without the use of vehicles.

Goal 7a: The Town should consider a variety of pedestrian friendly components in designing crosswalks at intersections in the Chaplin Initiative Area.

Goal 7b: The Town should require pedestrian 'safe havens' at the middle of a crosswalk in the median.

Goal 7c: The Town should remove the concrete sidewalks on both sides of William Hilton Parkway in the Chaplin Initiative Area and replace them with multi-use pathways separated a safe distance from the roadway.

Goal 7d: The pathway system in the Chaplin Initiative Area should link parks, open spaces and beach access facilities as well as residential and commercial facilities.

Need 8: Currently, only two beach access parks exist in the Chaplin Initiative Area, Driessen Beach Park and the informal beach access point at the end of Burkes Beach Road. The Town owns the majority of Chaplin Area properties, yet there is virtually no open space or parks in the area available for the public enjoyment.

Goal 8a: Provide a system of park, open space and other recreational facilities that take advantage of Chaplin Initiative Area's natural resources and location to attract recreational users.

Goal 8b: The Town should continue to build recreational parks, beach access parks and provide for open space and scenic vistas in the Chaplin Area.

Goal 8c: The Town should create a passive linear park system linking Town-owned lands and providing access to residential and commercial developments along the Broad Creek and extend to Shelter Cove.

Goal 8d: The Town should create a pedestrian link from the linear park system along the Broad Creek to the Ferguson Tract and Driessen Tract Parks.

Need 9: Sewer service is not established to all of the properties in the Chaplin Initiative Area. It is recognized that the use of septic tanks in the study area is a health and safety concern based on soil conditions and high water tables.

- Goal 9a:** Individual profit motivated developers should bear the expense and be responsible for the installation of sewer service to their developments, and be responsible to install service lines at an adequate size to service future needs in their area as determined by the Hilton Head Public Service District #1.
- Goal 9b:** The Town should investigate funding mechanisms that can front the funding needs for sewer installation and require property owners to reimburse their pro rata share of the costs of the sewer lines.
- Goal 9c:** The Town should facilitate grant acquisition with individuals and community groups to get grants for the extension of sewer service lines to income-qualified individuals.
- Need 10:** The State of South Carolina requires that all municipalities conform to fire protection that meets fire hydrant requirements of the 2000 International Fire Code. All parts of the Chaplin Initiative Area do not meet these requirements. Any redevelopment in the area must meet these codes for fire protection.
- Goal 10a:** The Town should enact ordinances that support the 2000 International Fire Code.
- Goal 10b:** Individual developers should be responsible to install fire protection water supply lines and fire hydrants that meet the future needs of Chaplin Initiative Area.
- Goal 10c:** The Town and the Public Service District should jointly find ways to assist income-qualified individuals who may be adversely impacted by these new fire code standards.

Chapter 5. IMPLEMENTATION STRATEGIES

The Chaplin Initiative Area Plan Implementation Strategies support and supplement the Comprehensive Plan and its elements. These strategies are provided for detail specific to the Chaplin Initiative Area. Firstly, though, implementation will require a three pronged effort.

1. LMO Amendments

The land use codes will be drafted, reviewed, revised and adopted based on the land use, density and design standards designations described in this Plan. A thorough study of the LMO is required to identify each revision needed, whether it is a zoning regulation or a design and performance standard.

2. Infrastructure

Integration of the proposed list of projects from the Infrastructure section into the existing Capital Improvement Program (CIP) is critical to the success of this Plan. There are several ways to finance these public projects; such as normal CIP funding; through the Tax Increment Financing District (TIF); as part of the cost of development (i.e. developer paid), or with grants and other outside funding sources.

3. The Chaplin/ Marshland/ Gardner Property Owners Association

Encourage the continued efforts of this neighborhood association and develop a relationship with the Town to mutually support the goals of this Plan. Property owners and stakeholders from the Chaplin Initiative Area need to have an input mechanism to provide information or feedback to the Town.

Implementation Strategies

Strategy #1. LMO Amendments

(See Figure #10, *Future Land Use* and Figure #11, *Proposed Zoning Districts*)

RM-8, Residential Moderate Density

RM-8 zoning classification is proposed to expand and include existing residential areas that were platted as long ago as the 1950s where new developments have been recently approved under similar density standards in the current RD, *Resort Development District*. Most of this property has already been developed as residential or is in the process of being developed that way. The RM-8 classification is used elsewhere on the Island when similar development has occurred or is being encouraged.

The areas set aside for RM-8 classification are all east of William Hilton Parkway. They include the platted subdivisions of Bradley Beach Road, Bradley Circle, Singleton by the Beach, and the remainder of the Seaside Subdivision. In the near future, the Town expects a residential subdivision on property between Singleton Beach Road, Hilton Head Resort and the Town-owned Malphrus Tract.

RD, Resort Development

For the property between the Atlantic Ocean and William Hilton Parkway, land not already residential or public should be zoned RD for resort development. Generally described, this includes the Burkes, Driessen, Ferguson, and Christopher family properties between Burkes Beach Road and the northern edge of the Chaplin Initiative Area. Certain conditions should apply to specific land uses in the RD district. Exceptions to this RD zoning designation are areas where land is best used as neighborhood scale commercial zoning.

NC, Neighborhood Commercial

The NC zoning classification encourages the continued use of neighborhood scale commercial activity along William Hilton Parkway near Singleton Beach Road as a replacement zoning classification of the current CL, *Commercial Light District* (See Figure #11, *Proposed Zoning Districts*). The NC District is intended to allow a cluster of businesses to serve the daily needs of the nearby residential areas.

PR, Parks, Recreation and Public Facilities

Public land should be classified PR to manage the types of land uses permitted on publicly held properties. The Town of Hilton Head Island is the largest single land owner in the Chaplin Initiative Area and the establishment of passive and active recreation areas while preserving natural features for public enjoyment are all integral to the goals of this Plan.

MM, Marshfront Mixed Use

This new zoning classification is proposed for lands between the marshes of Broad Creek and the arterial roads William Hilton Parkway, Mathews Drive, and Marshland Road, and replaces the current zoning classifications of the CL, *Commercial Light* and CMU, *Community Mixed Use Districts*. The MM zoning district would allow a mix of uses such as residential, commercial and resort accommodations at a scale suitable for the Chaplin neighborhood. Exact land uses will be determined during the amendments to the LMO. Residential above commercial would also be encouraged. Development should be oriented towards the marshes of the Broad Creek to take advantage of the Broad Creek marsh views. Suggested base density should be similar to the proposed CL currently in LMO amendment review stages. Bonus density provisions should encourage development to meet the goals of this plan.

Therefore this plan suggests bonus densities would apply when these criteria are met:

- 1) Combination of at least three parcels for a total acreage of more than 3 acres that allows appropriate road frontage and provides for better site design than the current narrow lots;
- 2) Elimination of curb cuts to no more than one for every 500 feet of frontage onto William Hilton Parkway;
- 3) Installation of or connect to sewer service;
- 4) Frontage on both William Hilton Parkway and the marshes of Broad Creek;
- 5) Providing either right-of-way or easements to adjacent properties, thereby allowing vehicular and pedestrian traffic access to multiple developments without needing to use William Hilton Parkway;
- 6) Allow for connection to the overall linear park plan;
- 7) Inclusion a storm-water retention system that increases retention beyond pre-development levels.

Design and Performance Standards

Design and performance standards of the Land Management Ordinance are also affected in the MM zoning district. Buffer and setback provisions pertaining to William Hilton Parkway, Mathews Drive, Marshland Road, freshwater wetlands and tidal wetlands will remain in effect. However, consideration should be given to reduction in buffer and setback standards applied from circulation streets. Buffer and setbacks between developments may be reduced to accommodate cross access and shared parking, however LMO provisions should accommodate the need to buffer between commercial and residential developments.

Development in this zoning district should be oriented towards the marsh views, as opposed to the William Hilton Parkway frontage. The scale and massing of the structures should be in keeping with the rest of the Island Character; their height should be below the treetops surrounding the building location. The buildings in this area should also endeavor to compliment one another in style and function. Finally, the location of the

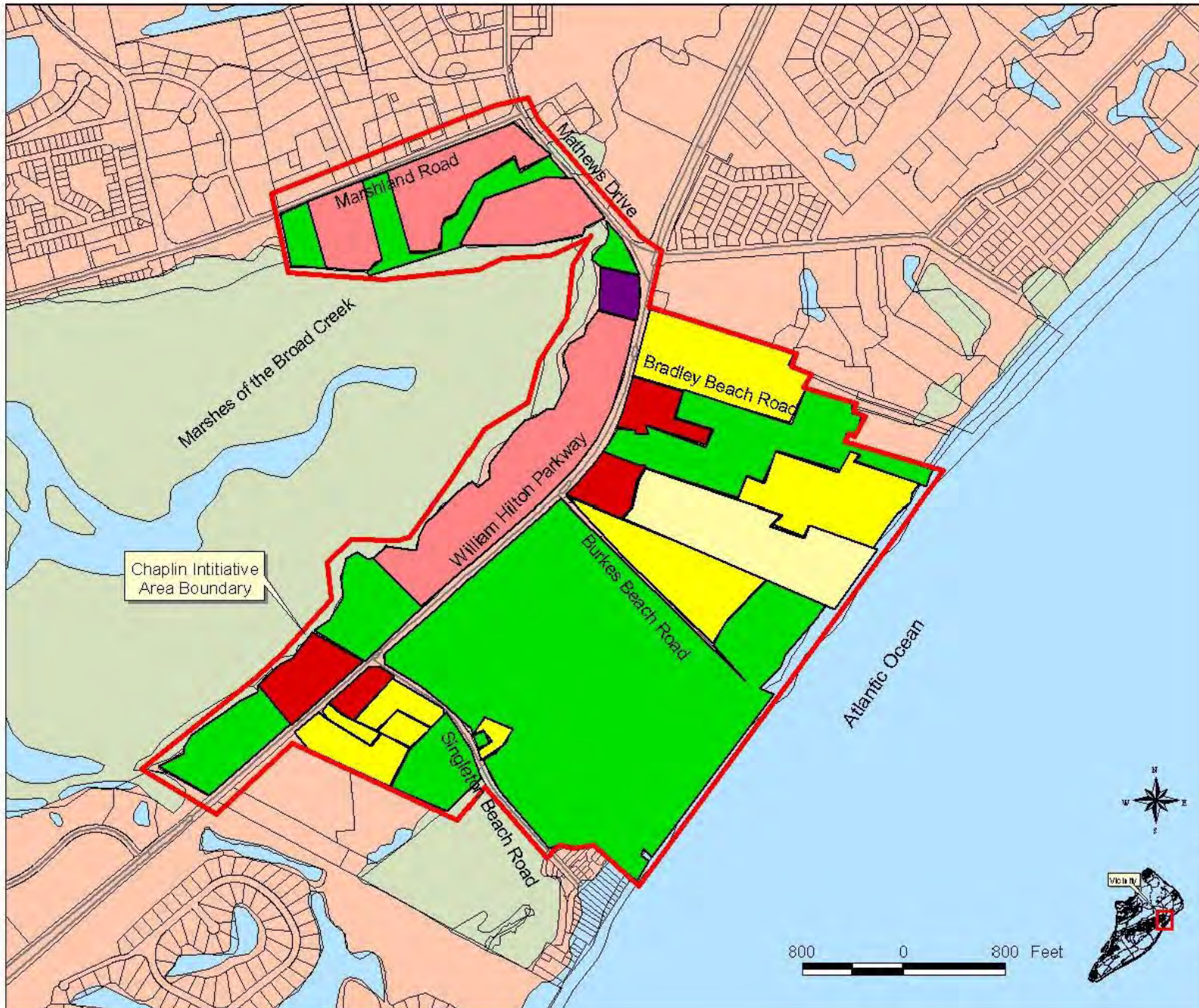
buildings should take into account the effect the building has on the view shed from developments across the marsh and boaters in the marsh.

In the area designated for the MM District there exists a remarkable opportunity to take advantage of the unique natural resources, such as stands of trees and marshfront views. In this zoning district, this Plan proposes to shape development around the existing stands of trees by allowing development and redevelopment to occur in the existing cleared areas with reductions in setback and buffer requirements between developments. The concept is that the existing stands of trees create natural buffer areas, and there is sufficient marsh frontage in the relatively cleared areas. In essence, the marshfront of the Chaplin Initiative Area should be master planned for natural resources and the development should be clustered where existing development and cleared areas already exist.

CON, Conservation District

Those portions of the study area that currently qualify as the CON District will remain under the CON designation. The purpose of the conservation district is to regulate very low intensity development in environmentally sensitive tidal wetland areas and the beach. Only development which will minimally disrupt natural features or systems, whether temporarily or permanently, will be allowed.

Uses allowed in the CON district are parks, utilities and other water-oriented uses.



Chaplin Initiative Area

Future Land Use

- Future Land Use
- Single Family
 - Residential/ Commercial Mix
 - Retail/ Commercial
 - Institutional
 - Resort
 - Open Space / Public
 - Main Roads
 - Parcel Lines
- Area Identity
- Open Water
 - Salt Marsh
 - Upland

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Figure # 10



Chaplin Initiative Area

Future Zoning Districts

Legend

Zoning Districts

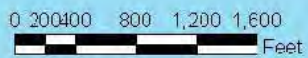
- CON
- MM
- NC
- PR
- RD
- RM-8

Other

- OPEN WATER
- SALT MARSH
- UPLAND
- Road Alignment
- Parcel Lines

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Strategy #2 Infrastructure Improvements

Roadway Improvements

Currently, the Chaplin Initiative Area is served primarily by the major arterial of William Hilton Parkway and the minor arterial roads Mathews Drive and Marshland Road. There are many curb cuts to properties situated along these heavily traveled roads, creating vehicle conflicts for those entering and exiting these roads. Vehicles are forced to enter the arterial roads in order to travel out of the area and within the area, even if the destination is next door. While recognizing that every property is entitled to a right-of-way, the Town intends to improve safety by eliminating curb-cuts through developer incentives and as opportunities present themselves during the construction of public projects.

Traffic calming measures are employed by the plan to help slow traffic speed and improve safety. For instance, landscaping the medians along the arterial roads tends to slow traffic due to a more closed-in view. A speed limit traffic study was completed in the Summer of 2001 which showed results that current speed limits do not need to be changed; however, residents of the area strongly encourage measures that will slow traffic speed.

A series of traffic calming and roadway improvements is proposed to mitigate the effect of so many access points along the arterial roads and to help slow traffic speed for safety.

- ◆ Encourage the elimination of curb cuts either through property combinations, consolidation, or removal of unused access points;
- ◆ Along William Hilton Parkway, the median area should be landscaped, providing for left turn and U-turn capabilities at appropriate intervals; (See Figure # 13, *Proposed Median Treatments*)
- ◆ Marshland Road should have a similar landscaped median as William Hilton Parkway; (See Figure # 13, *Proposed Median Treatments*)
- ◆ Powerlines and any other overhead utilities along the arterial roads should be buried; (See Figure #14, *Proposed Power Line Burying Projects*) and
- ◆ Marshland Road ditches should be covered and piped for drainage.

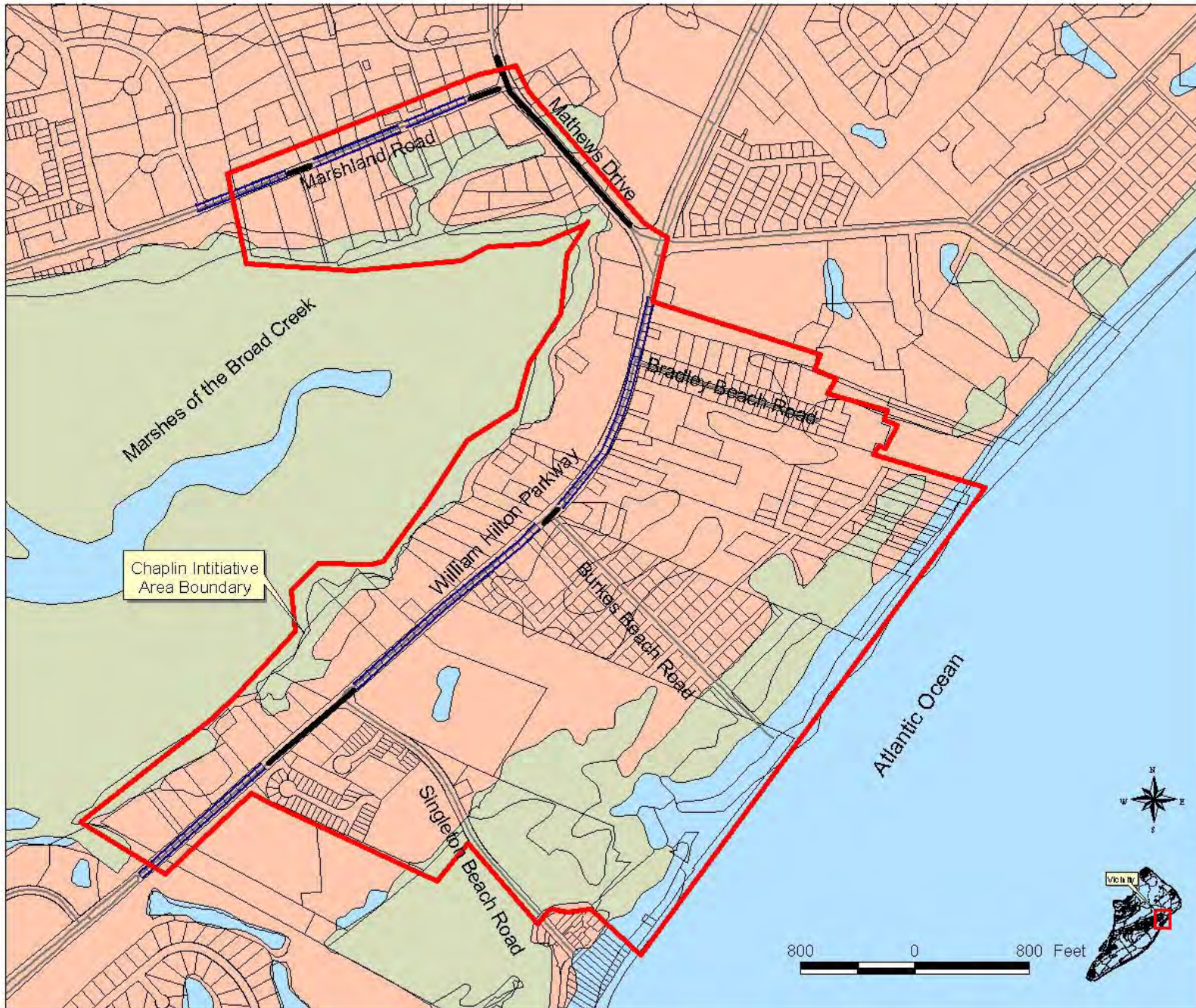
Figure #12, *Example of Median Landscaping (William Hilton Parkway)*



Existing William Hilton Parkway - 4 Lane with Paved Median










Proposed William Hilton Parkway – 4 Lane with Landscaped Median, sidewalk removed/ replaced by separated pathway.



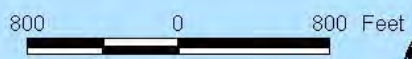
Chaplin Initiative Area

Proposed Median Treatments

- Median Treatments**
-  Landscaped
 -  Paved
 -  Main Roads
 -  Parcel Lines
- Area Identity**
-  Open Water
 -  Salt Marsh
 -  Upland

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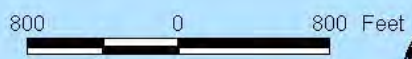
Chaplin Initiative Area

Proposed Power Line Burying Projects

- Main Roads
- Parcel Lines
- Overhead Power Lines
- To Be Buried
- Area Identity**
- Open Water
- Salt Marsh
- Upland

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New Roads

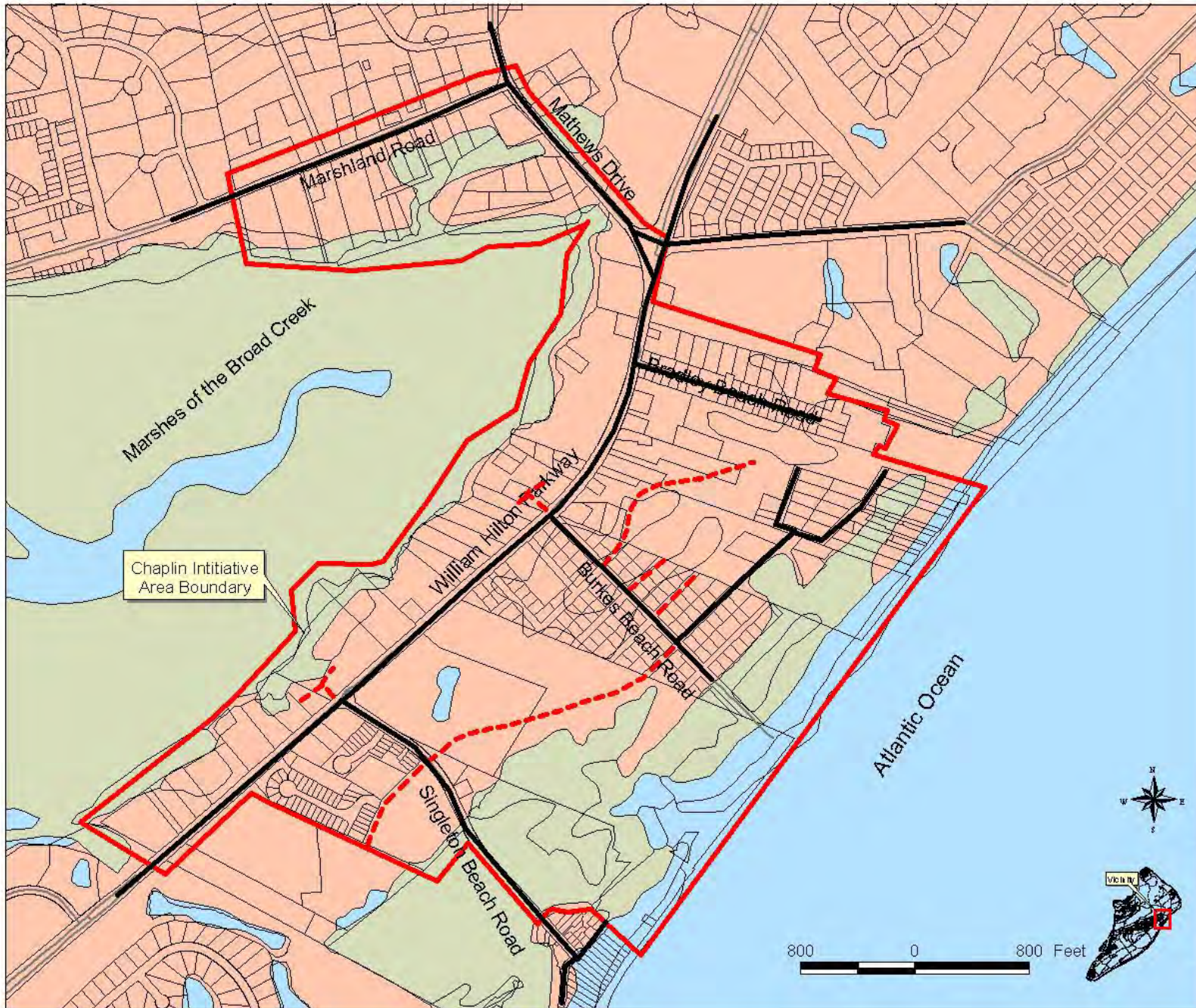
Between William Hilton Parkway and the Atlantic Ocean, several streets access William Hilton Parkway in an east-west orientation. These roads are Singleton Beach Road, Burkes Beach Road and Bradley Beach Road. There is no continuous north-south interior road to serve circulation between developments in this area and funnel traffic to fewer and safer intersections with William Hilton Parkway. Further, Bradley Beach Road's intersection is located only 850 feet from the Folly Field/ Mathews Drive/ William Hilton Parkway intersection, with a partially obscured view of on-coming traffic. Bradley Beach Road's access problem is acute since the Town of Hilton Head Island has a beach access park at the end of this road, which also serves as a residential street.

To create interior access to developments in this area, a new circulation road is proposed in a northeast-southwest orientation, roughly parallel to William Hilton Parkway. In the southern terminus, the road will connect with Hilton Head Resort, allowing a second access for this 500 unit development; allowing for the existing median closure on William Hilton Parkway and access to a potential traffic signal at Singleton Beach Road. At its northern end, this Plan originally proposed closing the intersection of Bradley Beach Road at William Hilton Parkway and providing only local residential traffic to the Bradley Beach homes via a new access connecting to this interior road. This road will also allow access to properties along William Hilton Parkway and allow the closure of curb cuts along this section of the Parkway.

The majority of Bradley Beach Road residents supported leaving the access of Bradley Beach Road in its current state with the addition of a pedestrian crossing at William Hilton Parkway. However, for safety reasons, Town Council voted to keep the closing of Bradley Beach Road's access to William Hilton Parkway as an option to consider when more detail projects are considered for the area.




Taking into account that the Town needs to gain a primary entrance to the Driessen Beach Park that is not via Bradley Beach Road, consideration should be given to gain access from Burkes Beach Road or directly off of William Hilton Parkway. Should the access to the beach park originate off of Burkes Beach Road, then the Town should consider the option of only one road crossing the Burkes Family properties.

Another interior road is proposed for the marshside of William Hilton Parkway to facilitate access between the developments in that area. This marshside interior road should align with either, or both, Burkes Beach Road or Singleton Beach Road as appropriate. (See Figure #15, *Proposed Road Plan*). Again, this road will also allow access to properties along William Hilton Parkway and allow the closure of curb cuts along this section of the Parkway.



Chaplin Initiative Area

Proposed Road Plan

-  Existing Roads
-  Future Roads
-  Main Roads
-  Parcel Lines
- Area Identity**
-  Open Water
-  Salt Marsh
-  Upland

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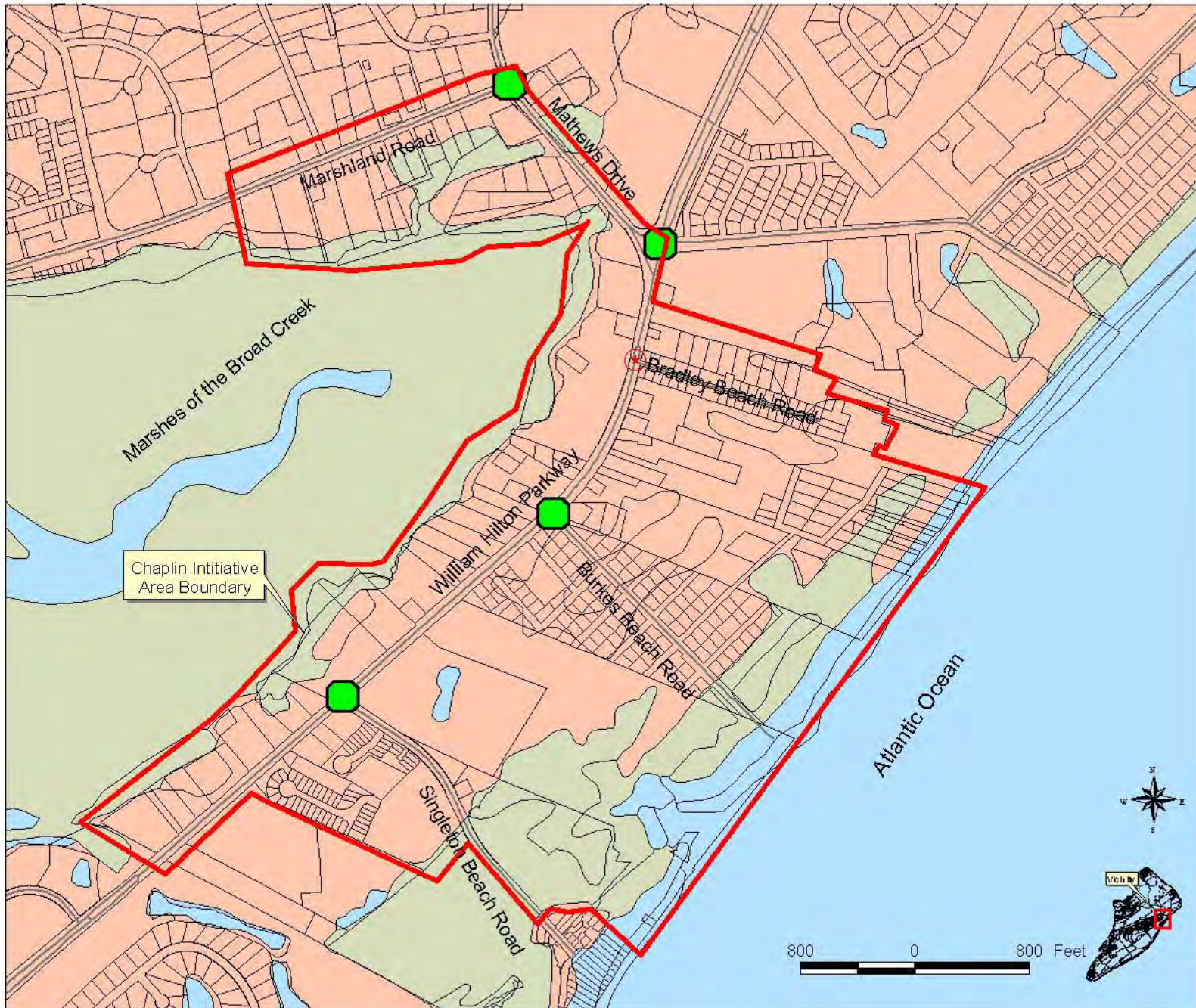
Traffic Signals

The only signalized intersection within the study area is at the Folly Field/ Mathews Drive/ William Hilton Parkway intersection. Working with SCDOT, traffic signal warrant studies are needed to determine the best location for any other traffic signals. Should the warrant studies prove that traffic signals are required, then the signalized intersection should have a set design that includes multi-phase, pedestrian friendly facilities. Warrant studies should be conducted for the intersections of Mathews Drive and Marshland Road; William Hilton Parkway and Burkes Beach Road; and William Hilton Parkway and Singleton Beach Road.

Intersection Improvements

As curb cuts are removed and consolidated, traffic will gravitate to the remaining intersections. Likewise, the inevitable development in the study area will increase not only vehicular but pedestrian traffic. With this thought in mind, this Plan proposes the installation of pedestrian friendly intersections at several major intersections.

Whether signalized or not, the following intersections with William Hilton Parkway should have crosswalks: Singleton Beach Road, Burkes Beach Road, Bradley Beach Road, at Hilton Head Resort, and north of the Folly Field/ Mathews Drive intersection. Crosswalk installation should occur at the Mathews Drive and Marshland Road intersection. In general, each crosswalk should be painted a color that warns drivers; include a planted median for a pedestrian safe landing; and textured paving should be located prior to the crosswalk to slow vehicles. (See Figure #16, *Proposed Intersection Improvements*).

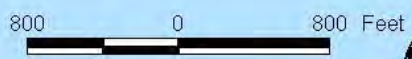


Chaplin Initiative Area

Proposed Intersection Improvement Locations

- Intersection Treatments**
- Improvements
 - ★ Crossing and/or Closing
 - Main Roads
 - Parcel Lines
- Area Identity**
- Open Water
 - Salt Marsh
 - Upland

April 2, 2002



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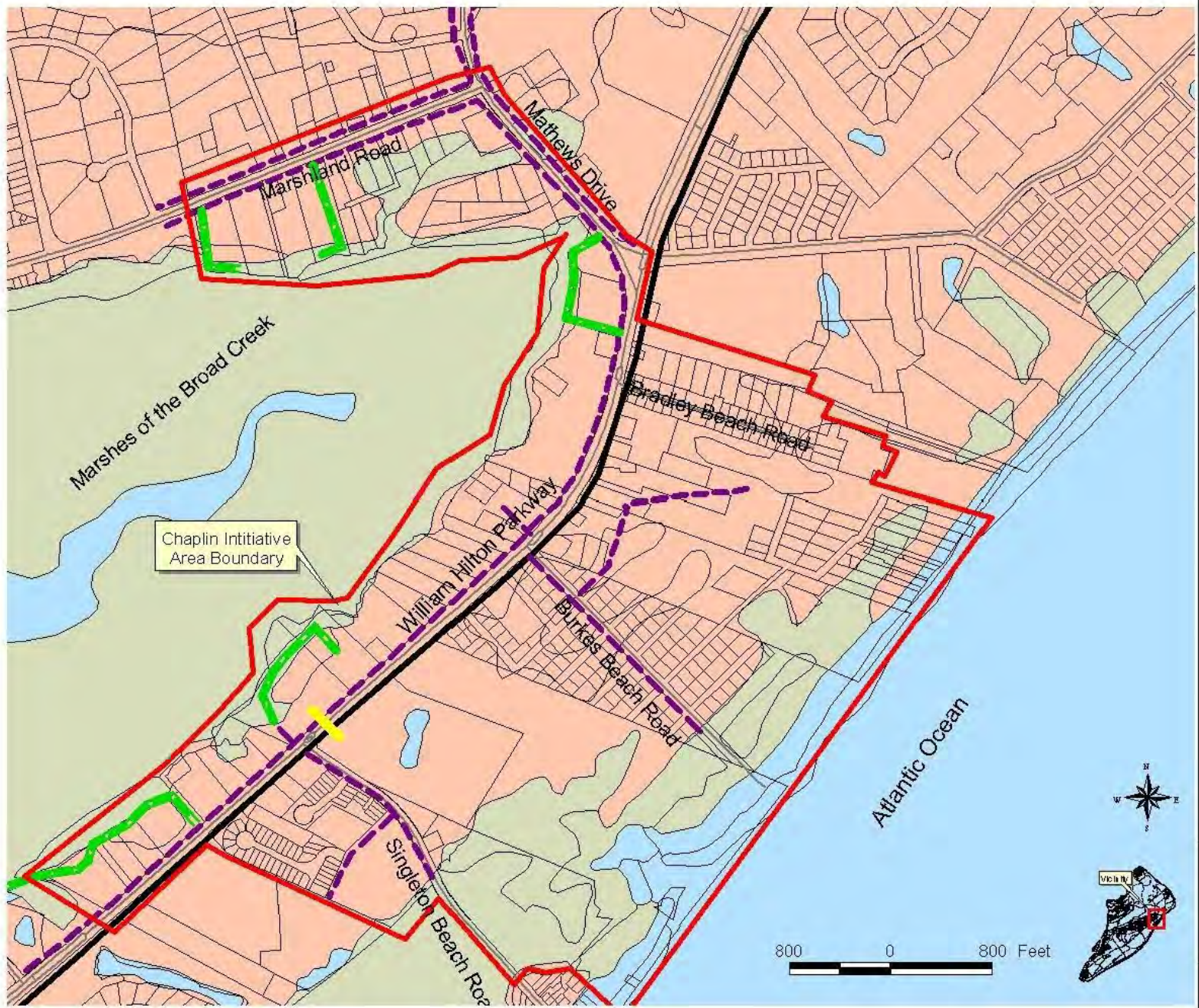
Pedestrian Improvements

Aside from the intersection treatments to improve pedestrian movements in the area, new and improved multi-use pathways are proposed throughout the Chaplin Initiative Area. Aside from the pathways located on the east side of William Hilton Parkway and on the south side of Burkes Beach Road, there are no other pathways located in the study area. Six foot wide sidewalks exist along both sides of William Hilton Parkway, but their use is uncomfortable for pedestrians.

In conjunction with the landscaped medians and pedestrian friendly intersection improvements, the sidewalks along William Hilton Parkway should be removed and a multi-use pathway should be installed on the marshside (west) of the road. In general, each major street or arterial road in the study area should have separated multi-use pathways on both sides of the road. (See Figure #17, *Proposed Pedestrian Facilities*).

After the charette, town staff considered the use of a pedestrian overpass across William Hilton Parkway in the vicinity of the Ferguson Tract Park. The concept of the overpass is primarily focused on creating connectivity with the linear park along the Broad Creek frontage that this Plan proposes to construct. While the efficiency and impact of a pedestrian overpass should be further studied, the hope is that this will add character and interest to the linear park and connectivity throughout the Chaplin Initiative Area that is safe.

In reference to lighting, this Plan proposes to install pathway lighting throughout the study area. The lighting should be designed to meet pedestrian needs as they use the pathway system in dark conditions. The only design feature specified during the charette was that the lighting should not exceed the height of a ballard sized fixture (street lighting was discussed during the charette, however, the neighborhood consensus is that street lighting is not necessary).



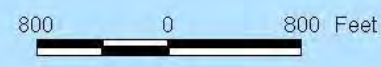
Chaplin Initiative Area

Proposed Pedestrian Facilities

- Future Pedestrian Facilities
- Existing Pathways
 - Future Pathways
 - Park Connector/ Broadwalk
 - Pedestrian Overpass
 - Main Roads
 - Parcel Lines
- Area Identity
- Open Water
 - Salt Marsh
 - Upland

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Public Facilities

The Town of Hilton Head Island owns approximately 161 acres of the 372 acres in the Chaplin Initiative Area. These properties are situated to take advantage of the ocean views with beach access and uninterrupted views of the marshes of Broad Creek. The properties are also spaced to provide separation between developments, creating natural buffers. Below, certain infrastructure improvements are proposed to facilitate those land uses for the public enjoyment.

The largest public facility combines several adjacent tracts under the project name of Ferguson Tract Park. Its location is generally described as the properties east of William Hilton Parkway, bracketed by Singleton Beach and Burkes Beach Roads. Currently in design and planned for construction in 2001, this park serves needs identified in the Recreation and Open Space Plan for a Recreation Complex combined with a neighborhood park and beach access facility. For the active portion of the park multi-purpose fields, tennis and basketball courts, playground equipment and a walking/jogging trail are included. Then aside from beach access with parking, passive areas of the park include access to The Folly tidal marsh, picnic areas and a dog walk facility. Linking the park to existing and proposed multi-purpose pathways and a crosswalk, or pedestrian overpass at the Singleton Beach Road/ William Hilton Parkway intersection encourages pedestrian access from the Atlantic Ocean and The Folly to the Broad Creek.

Creating linkage between the public lands in the Chaplin Initiative Area is an important element of this Plan. Once the multi-use pathways are in place along the arterial roads and major streets, all of the public land will be connected for pedestrian purposes. This Plan goes a step further, though. A combination boardwalk and multi-use path is proposed to link the public land fronting on the Broad Creek. This boardwalk/ pathway will create a linear park. Private developments should be encouraged to connect to the linear park, creating a much larger facility and public amenity. Also, the linear park should link with town properties located outside of the Chaplin Initiative Area, such as the Shelter Cove Tract immediately south of the study area. Care should be taken that this linear park does not cause undue harm to the natural resources while providing marsh view locations along its length. All of the public land in the study area fronting on the Broad Creek should be allocated to passive parks. A suggestion of a memorial park was discussed somewhere in this area.

The 1995 Recreation and Open Space Plan recommends developing a park on the Driessen Properties located between Bradley Beach Road and Burkes Beach Road. The park should be at a neighborhood scale. Access for the Driessen Beach Park should be through this area once the Bradley Beach Road intersection with William Hilton Parkway is removed and relocated to the interior circulation road. Also, this park should accommodate an expansion of the existing beach parking spaces in accordance with the Town's agreement with the South Carolina's Office of Coastal Resource Management to supply more beach access parking..

Another public facility that is needed in the Chaplin area is the installation of sewer lines. The Town should coordinate with the PSD to determine where to place the lines and the best size of the lines based on future land use.

Strategy #3 The Chaplin/ Marshland/ Gardner Property Owners Association

During the charette a number of ideas were presented that are clearly beyond the authority of the Town of Hilton Head Island to fully implement. Yet, the Town does have the ability and willingness to provide assistance in several ways. Below is a list of ways in which the POA could act as a clearinghouse, or go-between, for individual property owners and the Town.

- ◆ Research grant and other programs that can be tapped into by the Town to provide financial assistance to landowners trying to improve their property.
- ◆ The CDC or local community could work with local banks to set up low interest loans and other funding mechanisms to assist low income land owners with development that is consistent with this Plan.
- ◆ Provide the first step in assisting landowners with redevelopment of their property by having Current Planning staff meet with individuals to explain the regulations in the LMO and how they affect the developable area of their land.
- ◆ Work with the Native Island Business and Community Affairs Association and other community groups to establish a second step design assistance program. These groups could enlist local design professionals to meet with individuals to begin the design of the new development and redevelopment in a conceptual form. Final design and engineering phases or plans would be the responsibility of the landowners.

The POA could also create an unofficial architectural review board that provides guidance to individuals who wish to develop or redevelop property in the Chaplin Initiative Area. This notion requires some further guidelines to be established by the POA that are not detailed in this Plan.